

Richmond Downtown Streetscape

TOWN OF RICHMOND AND THE RICHMOND BUSINESS ASSOCIATION

Prepared by:

Kathleen Ryan - Landscape Architect
274 Maple Street, Burlington, VT

with

Arnold and Scangas Architects

&

Julie Campoli - Landscape Architects

Richmond Downtown Streetscape

TOWN OF RICHMOND AND THE RICHMOND BUSINESS ASSOCIATION

Prepared by:

Kathleen Ryan - Landscape Architect

274 Maple Street, Burlington, VT

p 863-4091 *f* 660-2939

kryan@together.net

with

Arnold and Scangas Architects

29 Ethan Allen Ave.

Colchester, VT 05446

p 655-1061 *f* 655-1068

Julie Campoli - Landscape Architect

23 Ledgemere St.

Burlington, VT 05401

p 865-0024

September 28, 1998

Table of Contents

I. Introduction.....	1
II Approach.....	1
III Existing Conditions.....	3
IV Recommendations	
Streetscape.....	9
Architectural.....	18
V Streetscape Renderings.....	24
VI Project Costs	33

I Introduction

Richmond's village center is a valuable asset to the community. It is home to several businesses and is a very active gathering place for residents of Richmond and the surrounding towns.

In order to maximize the potential of the village, town officials and the Richmond Business Association have initiated this study and several other related projects. The goal of this, the Richmond Downtown Streetscape Study, is to analyze the physical components of the downtown and recommend both short and long terms improvements that will enhance the appearance of the village center and make it an even more attractive commercial and social destination for its residents and the surrounding communities.

The project area is defined on the north by at a point about 250 ft. up the Jericho Rd., to the west at the rise in on Main Street coming into town, to the south by Railroad Street and to the east at a point 200 ft. beyond the corner market.

II Approach

Village centers throughout Vermont are threatened by large scale commercial businesses that have cropped up at their edges. It is clear that Vermont villages cannot provide the same choice of goods, or number of parking spaces provided by mega-stores. However, they can provide something else; a unique and friendly hometown environment that provides an inviting, comfortable and stimulating place for pedestrians to meet and shop. Richmond already has a lot of these qualities. To maintain and improve the downtown, this study recommends the following.

Analysis

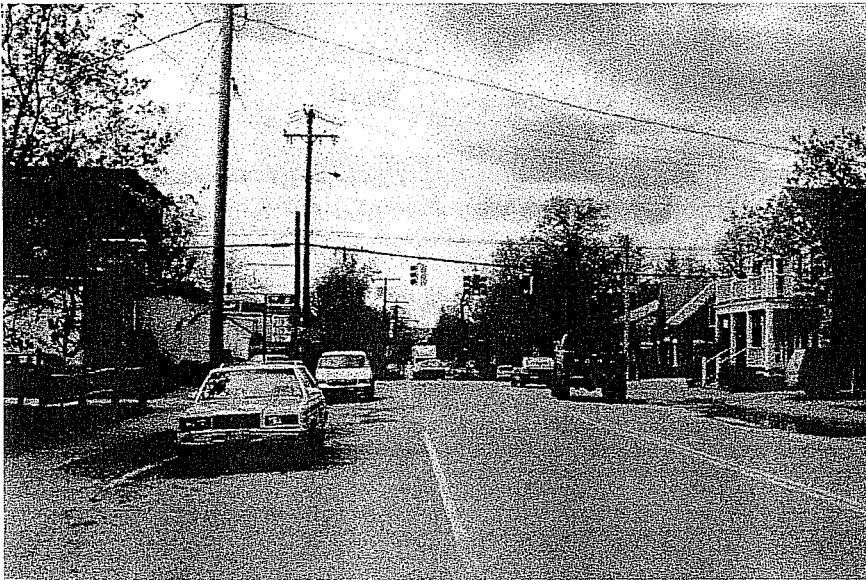
- Build sidewalks to connect all stores and businesses. Allow downtown customers and clients to park once and easily access stores, shops, banks and restaurants via comfortable walks.
- Provide shade trees where possible to add comfort and beauty to streets and sidewalk.
- Install planters/window boxes where possible on the commercial block and at other retail establishments to add seasonal color to the street. Maintain plants throughout the growing season. Replace with cut greens in winter.
- Define driveways to parking lots. Clear routes for vehicles provides safety and comfort for both pedestrians crossing driveways and drivers crossing sidewalks.
- Separate pedestrians from both roadway and parking lots. Screen the edge of parking lots located next to sidewalks with trees, and low shrubs that cover car bumpers or with fences or walls.
- Enhance building facades to reflect their rich details and the character of their period. Improve the appearance of buildings that are not compatible. Vermont's nineteenth century downtowns offer an unique historic setting that cannot be reproduced in new developments.

- Light the street, sidewalks and building facades in a manner that highlights the best qualities of the village center and creates an inviting nighttime setting.
- Use planting and lighting to enhance the streetscape and to emphasize the seasonal changes of the outdoor environment.
- Provide reasonable parking in a manner that respects the streetscape.

III. Existing Conditions

A photographic inventory was taken of the study area to determine existing conditions and areas where improvement could be made to the streetscape and adjacent buildings. The following photos, notes and maps indicate the findings. The site of each photo is keyed to the map that follows

#1 East Main Street



- The existing shoulder on the south side of the road is wider than necessary for parallel parking.
- A new sidewalk and greenbelt extend to the entry of the store. This greenbelt could support street trees.
- Sidewalk and greenbelt on north side in disrepair.
- Parallel parking on both sides of the road should be maximized for store and business clients, and apartment residents.

#2 The original porch of the historic corner store has been enclosed and is now used for storage. This building has potential for restoration.

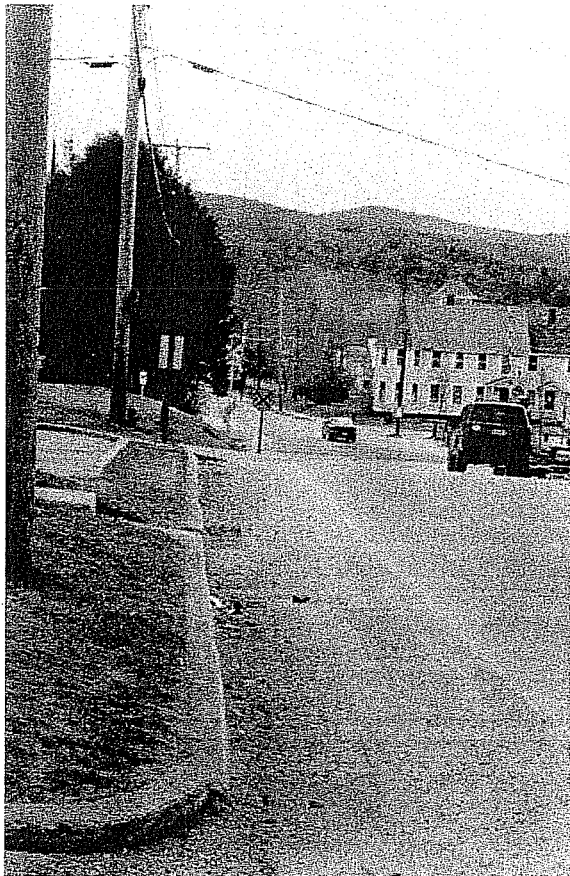
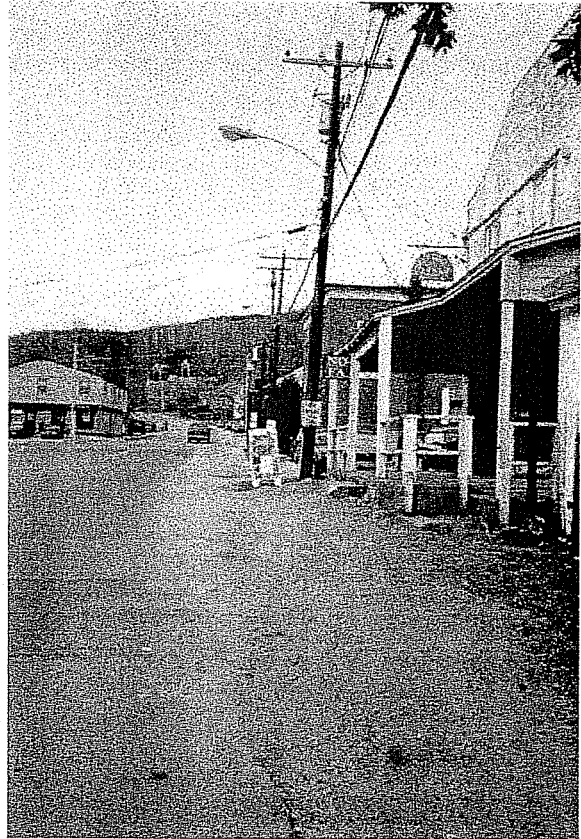
#3 Town Parking Lot

- Access drive to town parking lot is undefined.
- No sidewalk along street to bank & south side of village.
- Parking not screened from street.
- Street trees would help screen utility poles and lines.



#4 Bridge Street at the Daily Bread Building

- No sidewalk to access the porch and entry of this building.
- Unused roadway shoulder provides sidewalk space.
- Although in need of maintenance, this building has interesting and unusual features including the false front and tin siding.



#5 Bridge Street at Howard Bank

- The wide unused shoulder in front of the Howard Bank could be used to extend the sidewalk to the south end of the village, to provide a planted greenbelt for street trees or plants to screen parking in the bank parking lot. Parallel parking could also be provided in this area.

#6

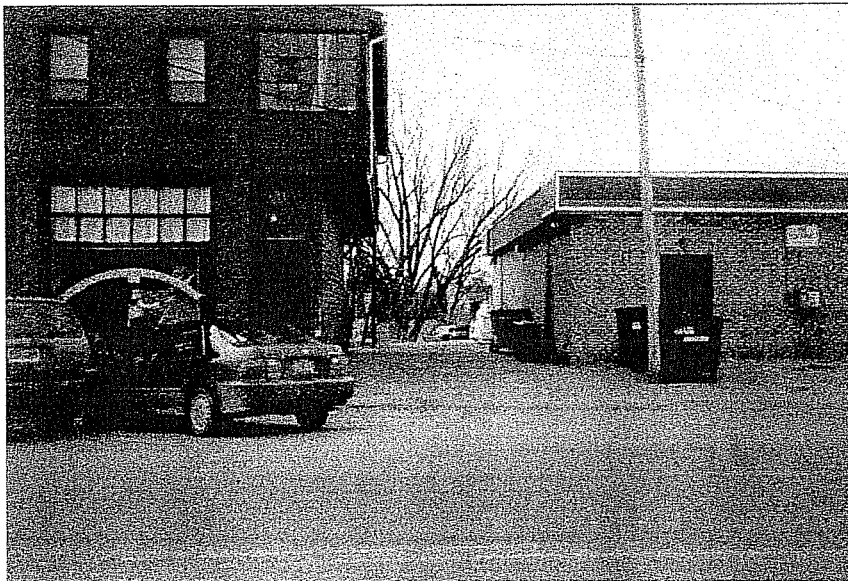
Bridge & Railroad Streets

- Corner of Bridge Street and Railroad Street is undefined.
- Missing sidewalk link from center of village to residential areas to the south.
- Street trees in green belt or on inside of sidewalk would screen utility poles.



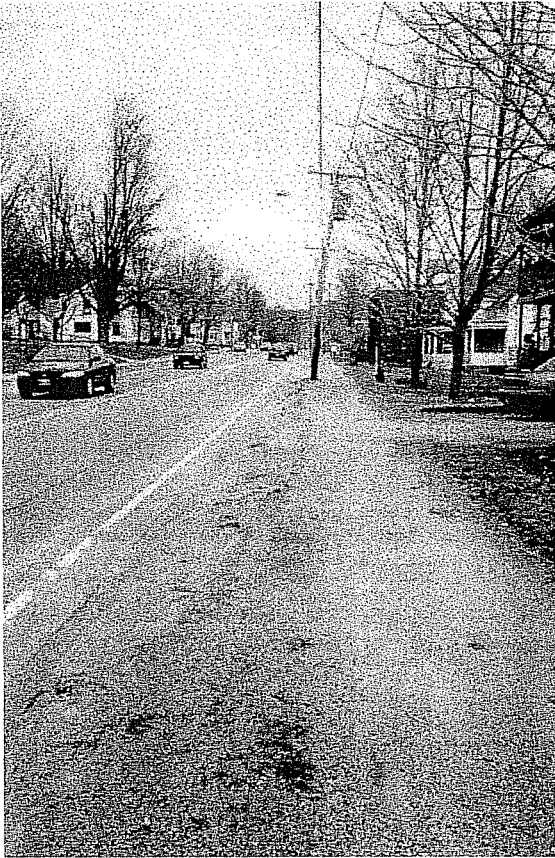
#7 Commercial Block

All of the buildings on this block have interesting architectural detail, some of which has been covered with contemporary materials. The sidewalk is fairly narrow for street trees. Street lights on this block are on the far side of the street do not illuminate sidewalk. Some storefronts have contemporary lights fixed to their facades.



#8 Bridge Street at Cumberland Farms

- View down utility alley, pole and dumpster on street detract from street and pedestrian environment.
- No sidewalk from corner to commercial block.
- No curb or access definition. Cars park in right-of-way.



#9 Sidewalk and on West Main

- Existing sidewalk and greenbelt in poor repair.
- Location of right-of-way not clear from plans available through town offices. Additional space for new sidewalk and planted greenbelt may be available within right-of-way.
- Street trees would provide screening for utility poles.
- Street trees would help define gateway to village and calm incoming traffic.

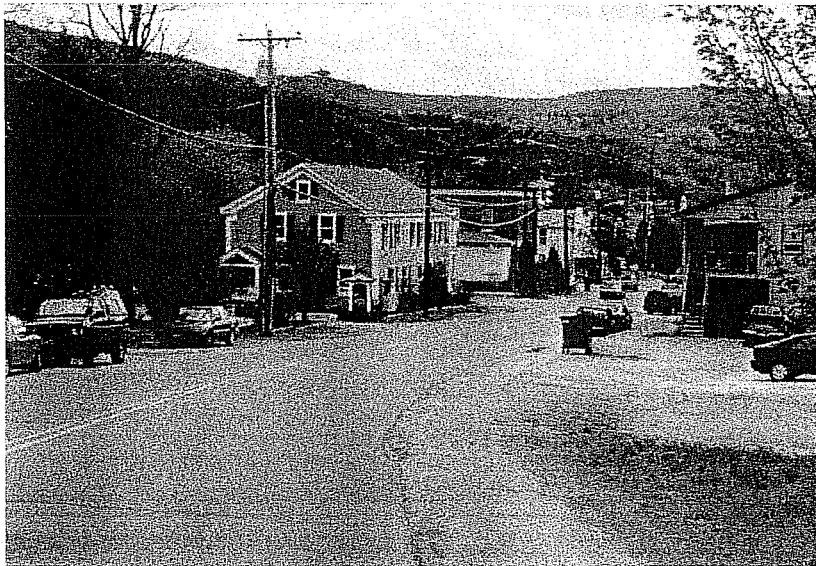
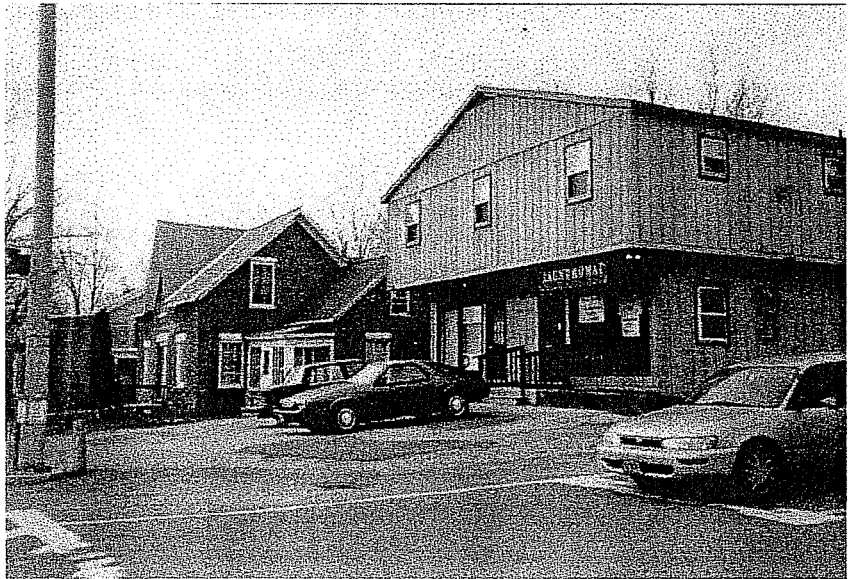
#10 West Main Street from Bridge Street to Baker Street

The north side of this street has a wide 7 foot shoulder that is not used for parking. The width of the roadway encourages high speeds. There is a relatively new sidewalk but no greenbelt on this side of the road. There are no street trees but many large trees on private lawns.

#11

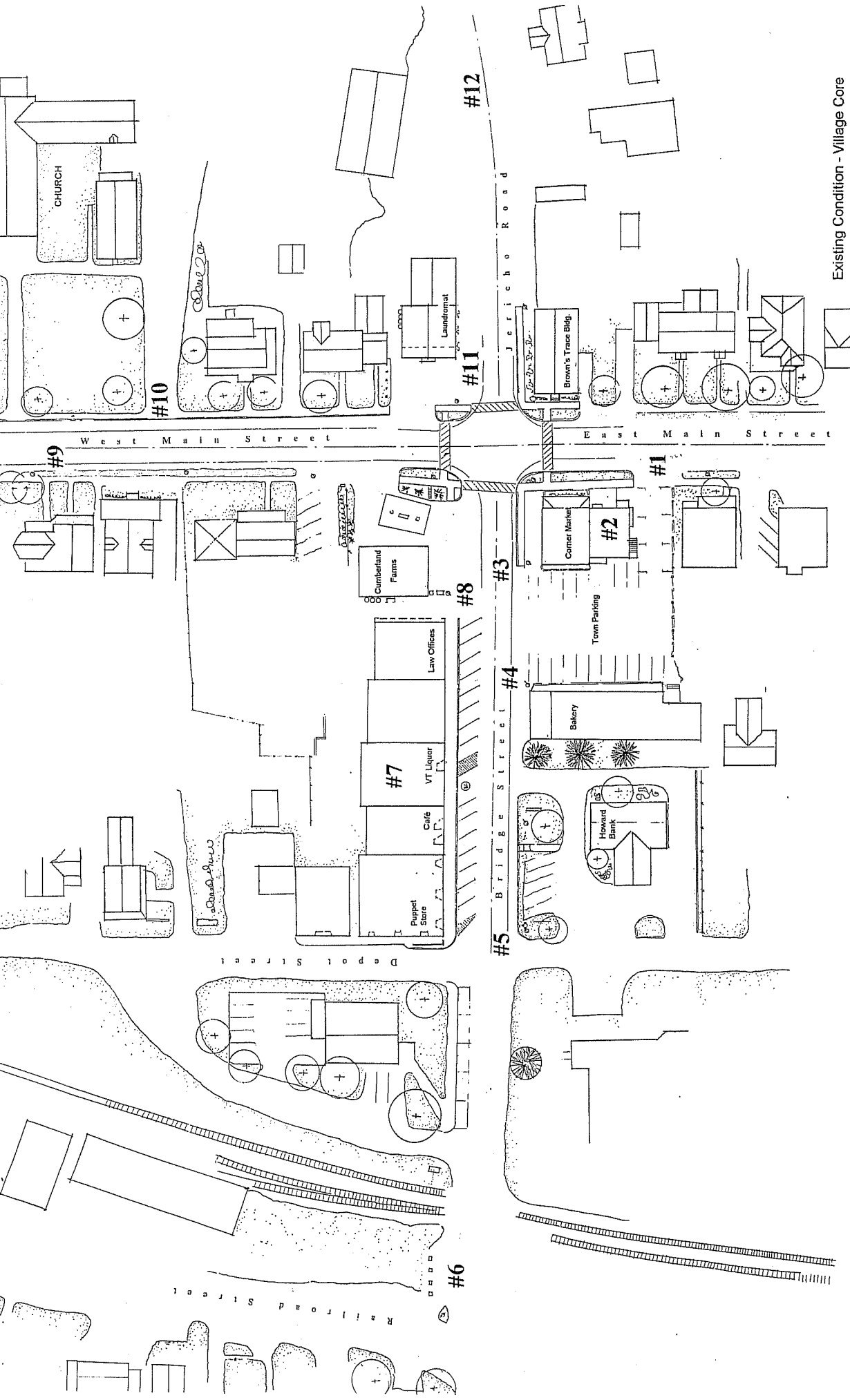
Main St. & Jericho Rd.

- Curb cuts poorly defined.
- Parking in front of building detracts from both building and streetscape.
- There is a small portion of sidewalk recently built at this corner.
- Sidewalk, greenbelt and planting needed to define a pedestrian space as separate from vehicle space.
- Building has potential for style and material improvements that would make it more compatible with other village architecture.



#12 Jericho Road

- This is an important village gateway for Jericho residents.
- Sidewalk and curb in poor repair.
- Dumpster in prominent location at edge of sidewalk detracts from streetscape and view entering village.
- Parking entry drive not defined.
- Parking lot not separated from sidewalk.



Existing Condition - Village Core
Richmond Downtown Streetscape
 TOWN OF RICHMOND AND THE RICHMOND BUSINESS ASSOCIATION
 Scale 1"=30'

Kathleen Ryan
 ARCHITECT
 110 STATE STREET, RICHMOND, VERMONT 05470

IV. Recommendations

Streetscape and architectural recommendations are noted on the following pages.

Streetscape

The plans, sketches and narrative on the following pages shows recommended improvements to the streetscape. Recommendation are shown both graphically and as notations on the plan. Larger scale originals of these drawings have been delivered to the Town Offices.

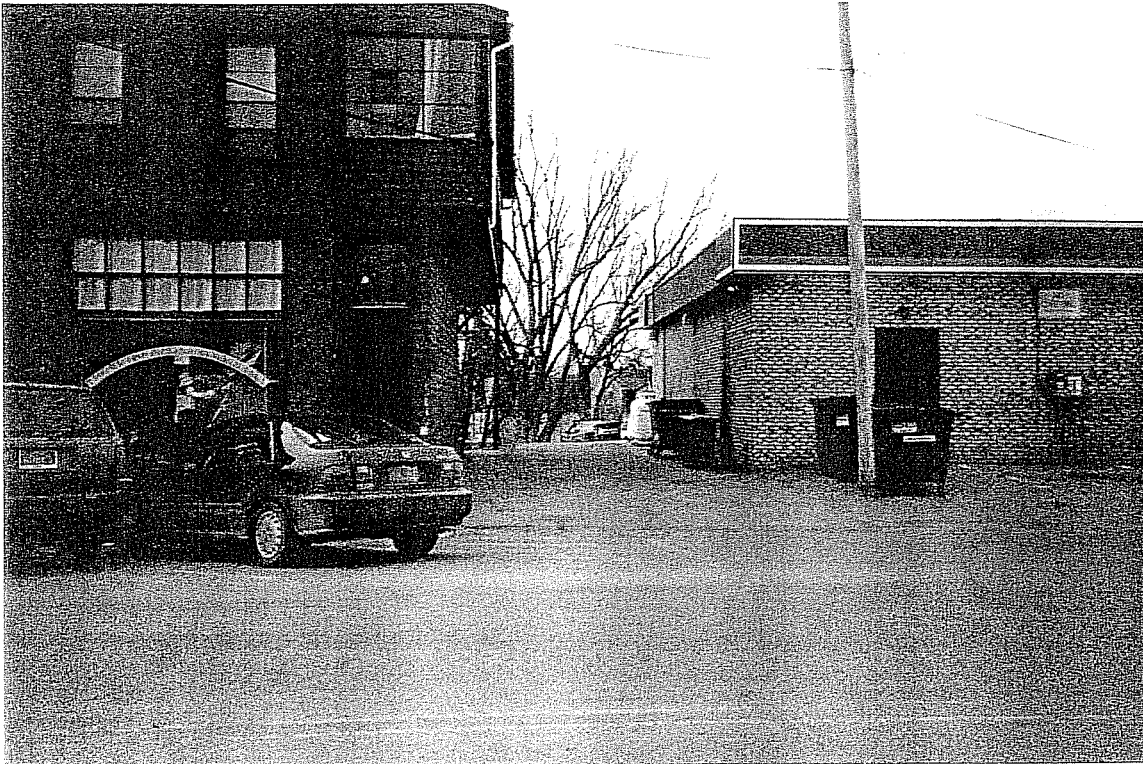
The first plan shows the village core. The second shows West Main Street between the west edge of the village and the church property.

Village Core

Parking is always in demand in small village centers. The diagonal parking in front of the commercial block as well as the town lot next to the Daily Bread Bakery is heavily used. A 25 space supplemental parking lot could be constructed on railroad property off Depot Street next to the tracks. An additional 8 diagonal spaces could be accessed off the railroad drive. These 33 spaces could be used for longer term parking and for employees of village businesses. The space behind the commercial block presents additional possibilities. This area, held by several landowners, will require further study.

Street Tree Planting: The center of the Village is defined primarily by its architectural character. (A later section of this report will address architectural recommendations for building facades.) On Bridge Street particularly, the street space is defined by the two and three story commercial block, the corner store and the Daily Bread building that sits very close to the street helping define the public space of that block. There is little area for green belts and planting immediately around the Rt. 2/Bridge Street intersection. However a few planting sites were identified.

1. Cumberland Farms - The Bridge Street side of this auto-oriented “quick-stop” is visually bleak and discourages pedestrians. There is no sidewalk connection across the parcel and in fact cars are usually parked across the pedestrian route. In addition, dumpsters are located next to the sidewalk route. The photo and sketch below show existing conditions and recommendations for this location



West Side of Cumberland Farms - No pedestrian route from commercial block to corner. Dumpsters detract from streetscape.



Proposed Green space and sidewalk added to Bridge Street right-of-way- Tree provides shade and screens pole. Hedge (or fence) screens dumpsters. Sidewalk indicates pedestrian route.

2. Bridge Street & The Town Parking Lot - Recommendations for Bridge Street include a side walk from Railroad Street to the existing sidewalk, which will provide a pedestrian link for the new residents of that area. Tree islands, defined parallel parking and planters are also suggested. The Town parking lot could be improved by the addition of a sidewalk on the street-side that provides a comfortable route for pedestrians from the Main Street intersection to the south end of Bridge Street. Tree islands would separate the cars from the pedestrian way and screen the expanse of the parking lot from the street. Paving, striping and handicapped signs would maximize the use and convenience of the lot.

3. West Main Street

West Main Street (Route 2) is a state highway with a functional classification of Major Collector. In past years, when such roads were renovated, they were widened, to widths that were determined by federal standards----- up to 40 feet, (12 foot lanes and 8 foot shoulders). Last year the Vermont Agency of Transportation issued Vermont State Standards for roadway design that were geared to local conditions and needs. These standards allow for much narrower roadways through village centers. Minimum lane and shoulder widths vary according to classification, traffic volume and speed but flexibility allows road designs to adapt to a downtown, pedestrian setting. Minimum dimensions for East and West Main Street are 11ft for lanes and 3 to 4 feet for shoulders. These widths leave adequate room for cyclists to share the road.

The recommendations in this report that show a sidewalk, greenbelt, and street trees take advantage of the new regulations to narrow the road and use the acquired asphalt width for greenbelt. Keeping the roadway to a width of 28 feet has other benefits. Narrow, tree lined roads have been shown to reduce traffic speeds more effectively than speed limit signs.

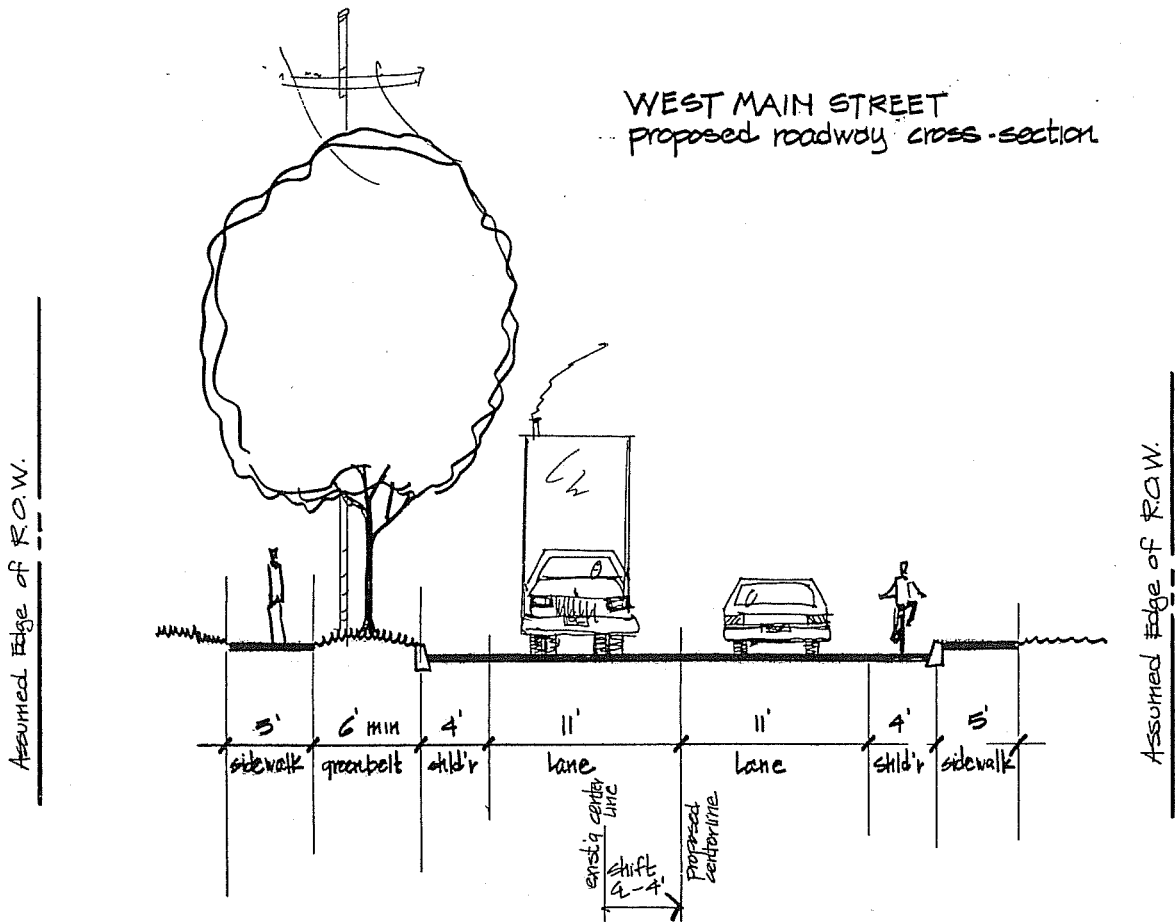
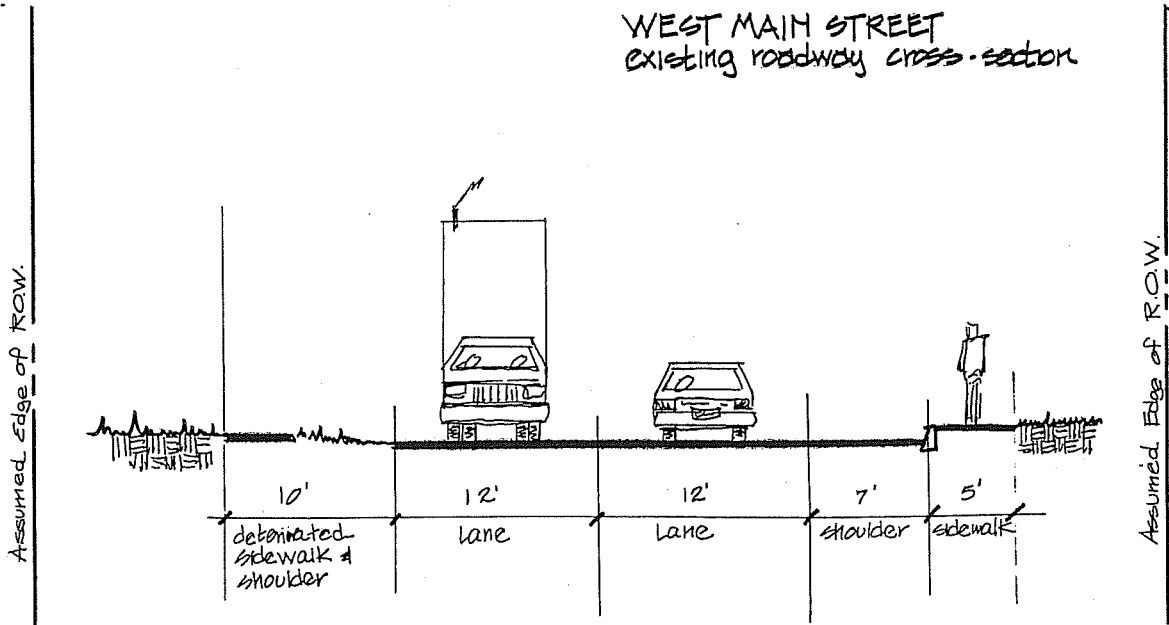
The West Main Street plans show how the right-of-way can accommodate the existing sidewalk on the north side of the road, and a new planted greenbelt and sidewalk on the south side of the road. This plan is based on sewer plans acquired from the town, on aerial photos and from field dimensions. A survey will be needed to establish the actual limits of the right-of-way and the location of the roadway within that right-of-way. Further engineering work and negotiations with Vermont Agency of Transportation officials will also be needed before a final roadway design is accomplished. However it is clear that a village style streetscape is possible along this segment of roadway.



South side of West Main Street - Existing Conditions

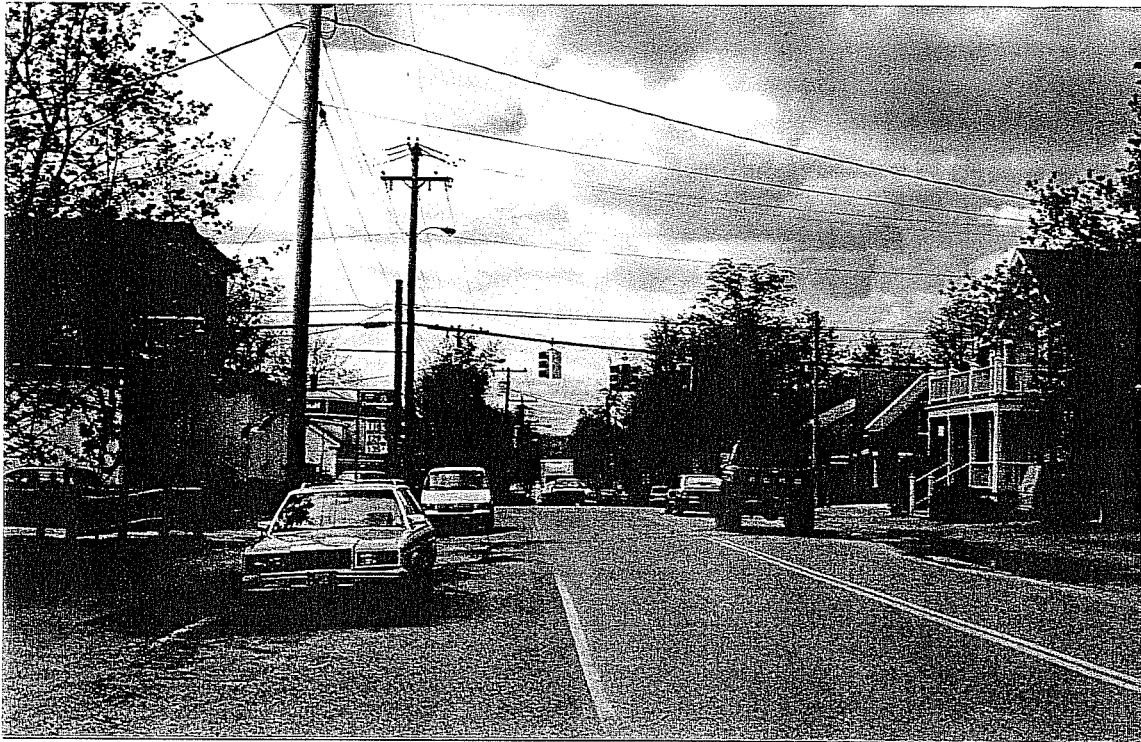


West Main Street - New sidewalk, expanded greenbelt and street trees. Trees minimize utility poles and lines



Proposed Roadway Improvements

4. East Main Street - Width of road and right-of-way allows for parking, sidewalk and some greenbelt



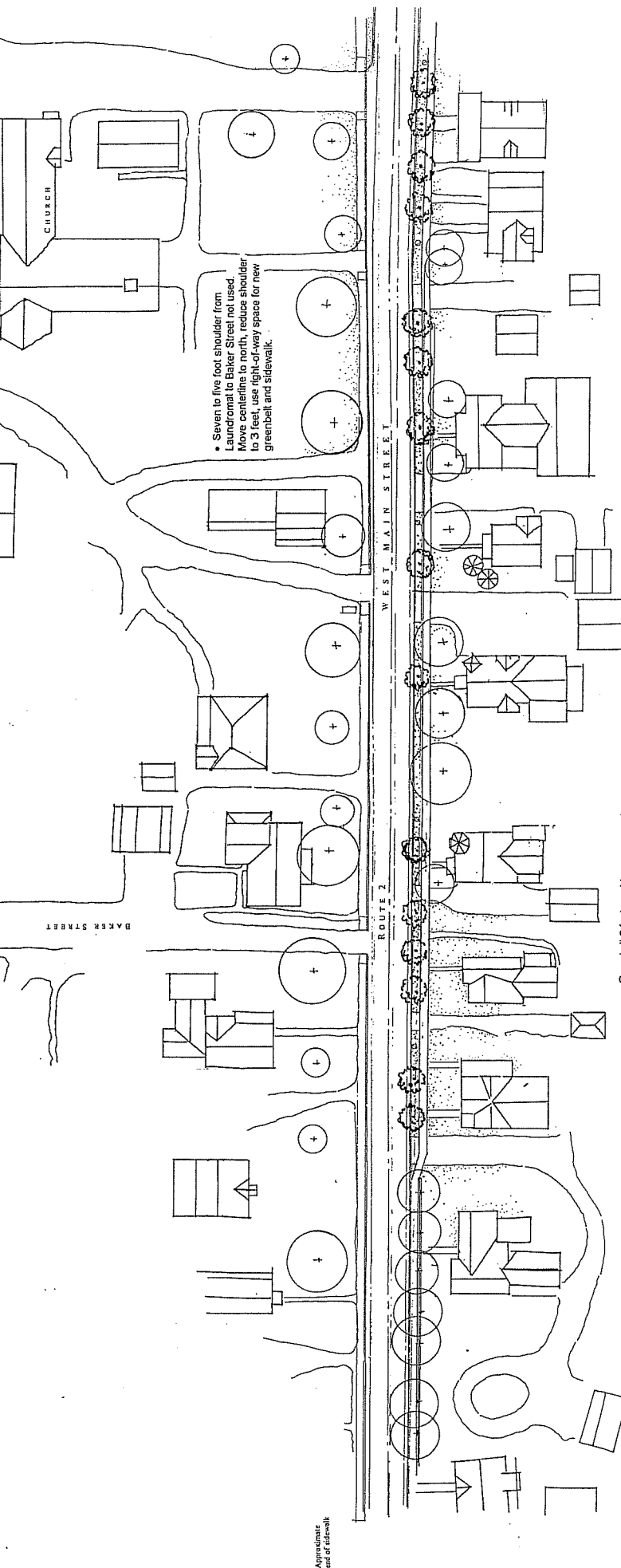
South side of East Main Street - Existing Conditions



West Main Street - New sidewalk and parking on north side, parking remains on south side. Greenbelt, tree islands and street trees possible in many locations.

5. Jericho Road - This well traveled entry to the Downtown could be enhanced with the addition of a sidewalk and landscaped greenbelt on the west side of the road. Street trees will provide a canopy for pedestrians and focus the entering view into the center of the town. There is an existing, badly deteriorated asphalt sidewalk that extends along the right-of-way several hundred feet up the hill. Through agreement with the adjacent property owner, recommended improvements that bridge the right-of-way line, including those on the Jericho Road-Main Street corner could be accomplished, improving both the streetscape and bordering private site.

6. Railroad Parking Lot - Railroad property may be available to the town to supplement the limited downtown parking. Construction of a 25 space lot together with 8 new diagonal spaces off the railroad driveway would almost double the downtown spaces now available in the town parking lot and in front of the commercial block. Business owners and their employees would be encouraged to use these spaces freeing up spaces nearer to commercial storefronts for customers.



• Seven to five foot shoulder from Laundromat to Baker Street not used. Move centerline to north, reduce shoulder to 3 feet, use right-of-way space for new greenbelt and sidewalk.

Adjustments
and of sidewalk

• Greenbelt 6 feet or wider can support street trees. Add trees where they will not conflict with existing trees on residential lawns.

• Jog sidewalk as needed to accommodate trees and walls in front of residences.

• Establish the location of edge of right-of-way by legal survey.

North Main Street

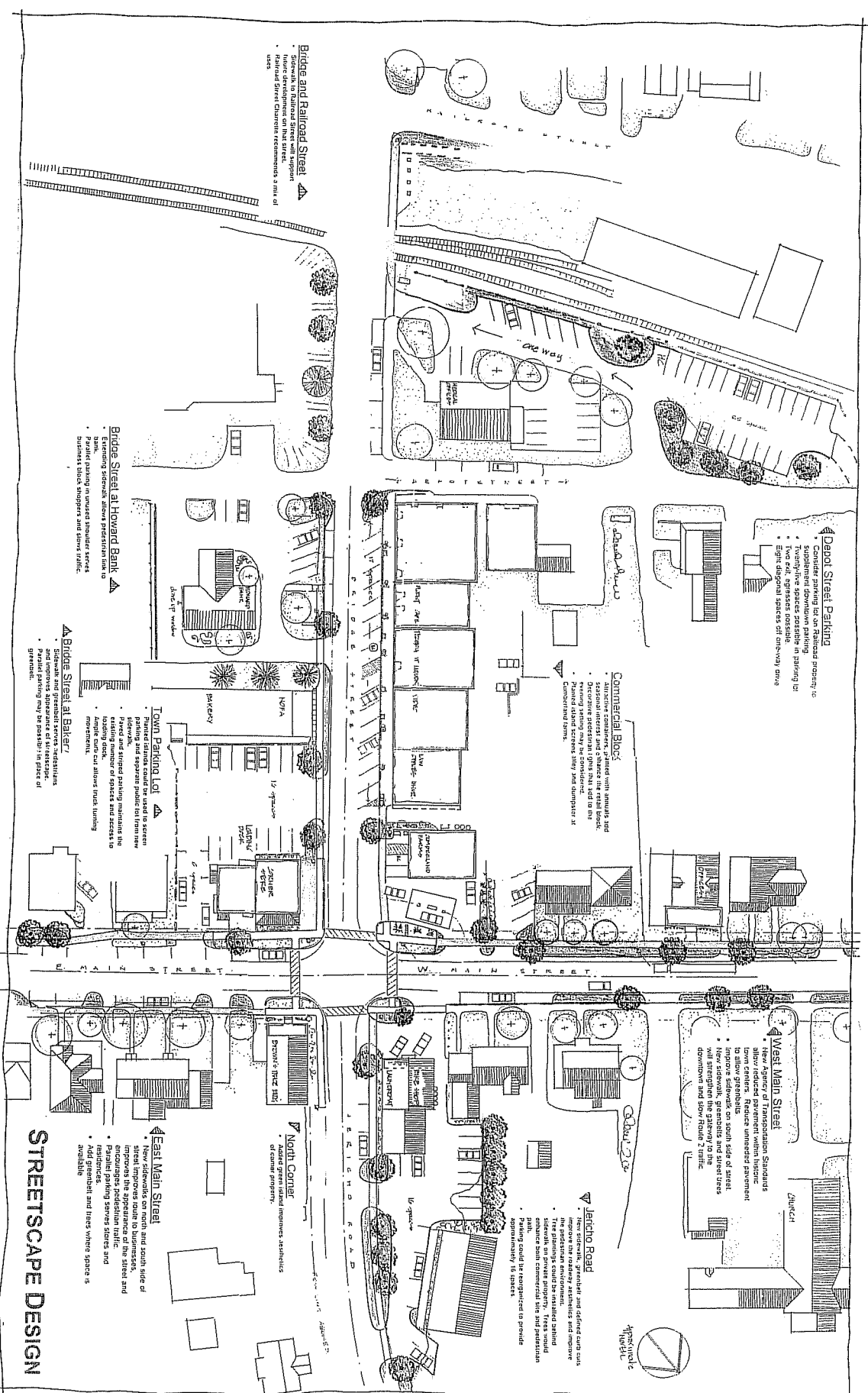
- New Agency of Transportation Standards allow reduced pavement within historic town centers - Reduce unneeded pavement to allow greenbelts.
- New sidewalk, greenbelts and street trees will strengthen the gateway to the downtown and slow Route 2 traffic.

Existing Conditions and Recommendations - North Main Street

Richmond Downtown Streetscape
TOWN OF RICHMOND AND THE RICHMOND BUSINESS ASSOCIATION

Establish Bylaws





Depot Street Parking

- Consider parking lot on railroad property to:
- Provide five spaces possible in parking lot
- Two exit, separate possible
- Eight diagonal spaces off on-way drive

Commercial Block

- Attractive commercial, painted with murals and seasonal accents and colors in the fall
- Evening lighting may be considered
- Painted street screens, sign and dumpster at commercial block

Bridge Street at Howard Bank

- Parallel parking in unobstructed areas
- Painted curb allows truck turning

Bridge Street at Baker's

- Sidewalk and street furniture
- Parallel parking may be possible in place of driveway

Town Parking Lot

- Painted islands could be used to screen parking and separate public lot from new parking
- Paved and striped parking maintains the existing number of spaces and access to ample curb cut allows truck turning

West Main Street

- New Agency of Transportation Standards allow reduced pavement width historic to allow greenbelts
- New sidewalk, greenbelts and street trees
- New sidewalk and street trees down town and along Route 2 traffic

Jericho Road

- New sidewalk, greenbelts and defined curb cuts improve the roadway aesthetics and improve pedestrian environment
- New sidewalk, greenbelts and street trees enhance curb commercial site and pedestrian
- Parking could be incorporated to provide approximately 15 spaces

North Corner

- Added green island improves aesthetics of corner property

East Main Street

- New sidewalk on north and south side of street improves route to businesses, and improves the appearance of the street and residences
- Parallel parking serves stores and residences
- Add greenbelts and trees where space is available

Bridge and Railroad Street

- Sidewalk to Railroad Street will support
- Railroad Street Character recommends a mix of uses

STREETSCAPE DESIGN

Richmond Downtown Streetscape
 TOWN OF RICHMOND AND THE RICHMOND BUSINESS ASSOCIATION
 Scale: 1"=30'
 May 6, 1998

Established Type
 ...

Architecture

Richmond's village center has an excellent resource of historic buildings. Although contemporary buildings occupy two prominent corners of the Main Street/Bridge Street intersection, many turn of the century residential buildings on Main Street and the historic commercial block on Bridge Street still predominate and establish the architectural character of the village center. The renovation of several village building in recent years has strengthened the image of the downtown. Renovations have occurred at many residences, bed and breakfast establishments, the Goodwin Baker building, and the Browns Trace building. During the period of this study, renovations have begun on the Laundromat building.

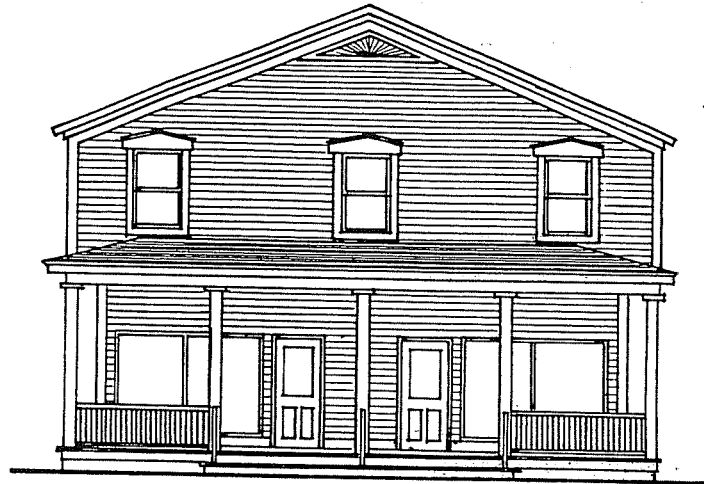
Historically the commercial blocks of most Vermont towns were not planted with street trees. Trees did line the streets in residential areas and were planted in town greens. The architectural detail of commercial and civic building lent beauty to the downtown blocks these buildings occupied. Returning the architectural detail to Richmond's commercial block will be important to improving the downtown. Historic materials, architectural interest and variety will create a lively visual setting that will strengthen Richmond's commercial center.

In many cases relatively modest facade improvements can dramatically enhance a building. The town may be able to assist property owners in identifying funding and tax incentives available for downtown building renovations.

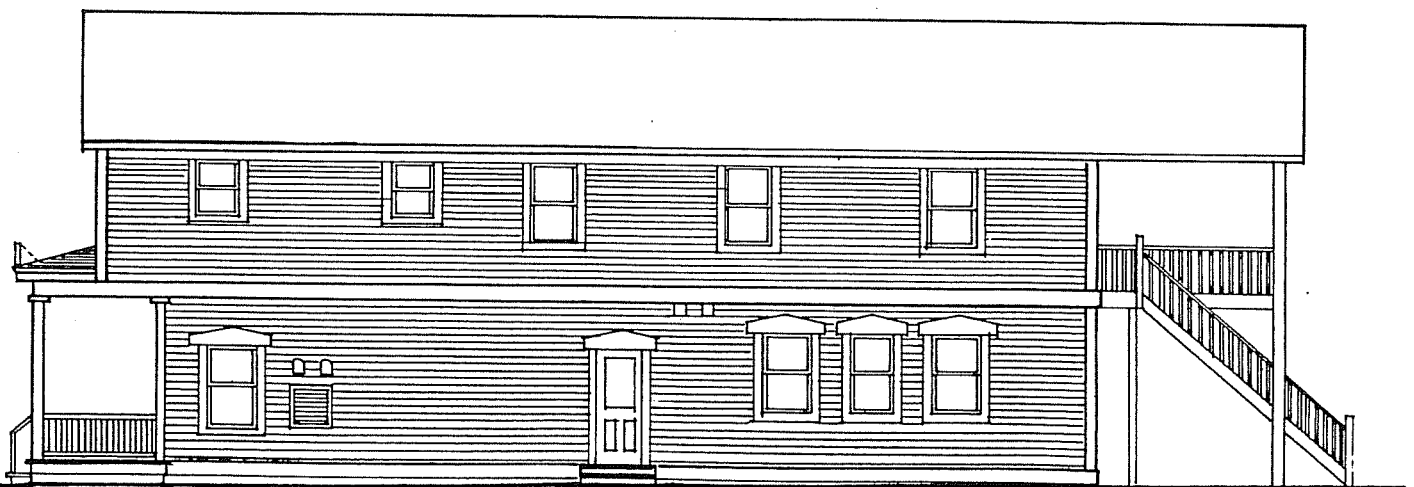
The following pages suggest possible architectural treatments to two prominent free standing buildings and the row of buildings on the commercial block. These sketches represent a set of facade improvements that may be useful to owners who are be considering renovations. The recommendations are all compatible with the traditional architecture of the community.

Laundromat

- Replace vertical siding with horizontal clapboard. Add corner trim boards.
- Replace window trim with wider boards and “pitched” header.
- Replace roof vent with fan shaped vent.
- Build new porch entrance to stores.
- Establish appropriate, uniform signage at edge of porch roof.
- Box out columns at rear of building.



LAUNDROMAT - MAIN STREET



LAUNDROMAT - JERICO ROAD

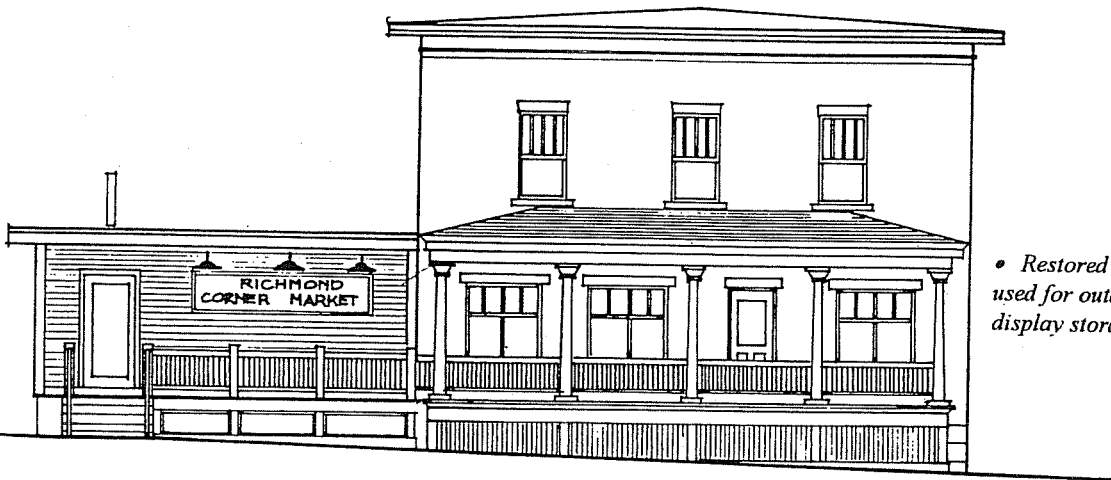


Tourists Hotel - Built 1923 - ca. 1931

Historic Photo of the Corner Market building (circa 1931)-
Formerly the Tourist Hotel and Richmond Lunch

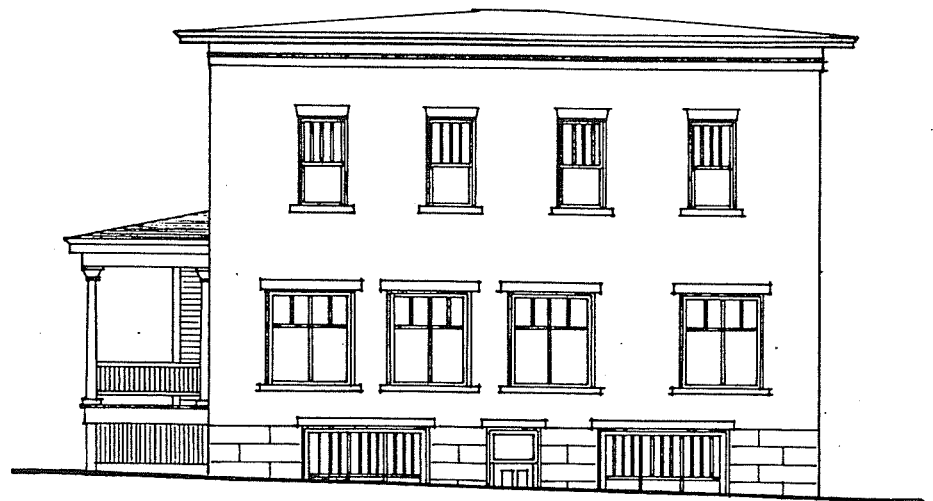
Corner Market

- Remove siding from original porch and restore porch as per historic photo.
- Remove boards from first floor windows and restore window sashes similar to historic photo. Install black window glass or paint back of glass black. Infill behind window glass with insulated walls.
- Install new clapboard siding over the concrete block structure.
- Build new deck an entrance and connect to restored porch.
- Remove plastic from upper story windows and install new aluminum or interior storm windows.
- Paint wood trim at windows and cornice.
- Provide new sign for business with period lighting above.



- Restored and new porch could be used for outdoor seating and/or display storage of products.

CORNER MARKET - MAIN STREET



- If window glass is to be painted, consider painting windows with faux images of grocery items; vegetable, fruit, breads etc. before black background is added.

- Remove large shrubs. Maintain green space with low shrubs that show building form and detail

CORNER MARKET - BRIDGE STREET

Commercial Block

Masonic Temple Building

- Remove siding on upper windows. Block windows as per puppet store as required to cover interior drop ceiling
- Remove rail at puppet store.
- Second floor windows- replace in-fill windows with traditional tall windows or paint wood in-fill dark color to blend with brick.
- Design sign to fit sign board trim or install hanging sign above sidewalk.

Liquor Store and Pizzeria

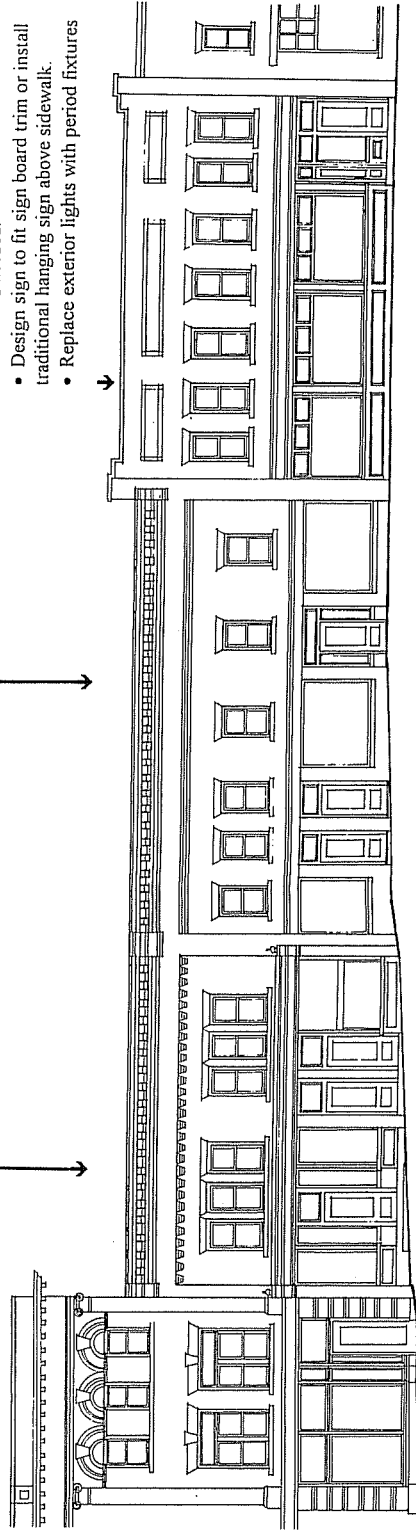
- Remove siding and open up windows.
- Open upper windows, install black or painted glass to block dropped ceiling.
- Replace door with period style salvage or reproduction.
- Design sign to fit sign board trim and/or replace existing hanging sign with traditional style hanging sign.
- Replace exterior lights with period fixtures

Video Store

- Remove siding from lower facade Open up windows. Use shades, blackened glass as required to screen interior functions.
- Open up, paint and repair wood cornice as required.
- Replace bead board at entry and paneling below windows as needed.
- Design sign to fit sign board trim or install traditional hanging sign above sidewalk.
- Replace exterior lights with period fixtures

Café and Bear Hug Florist

- Open upper windows at Café.
- Replace exterior lights with period fixtures



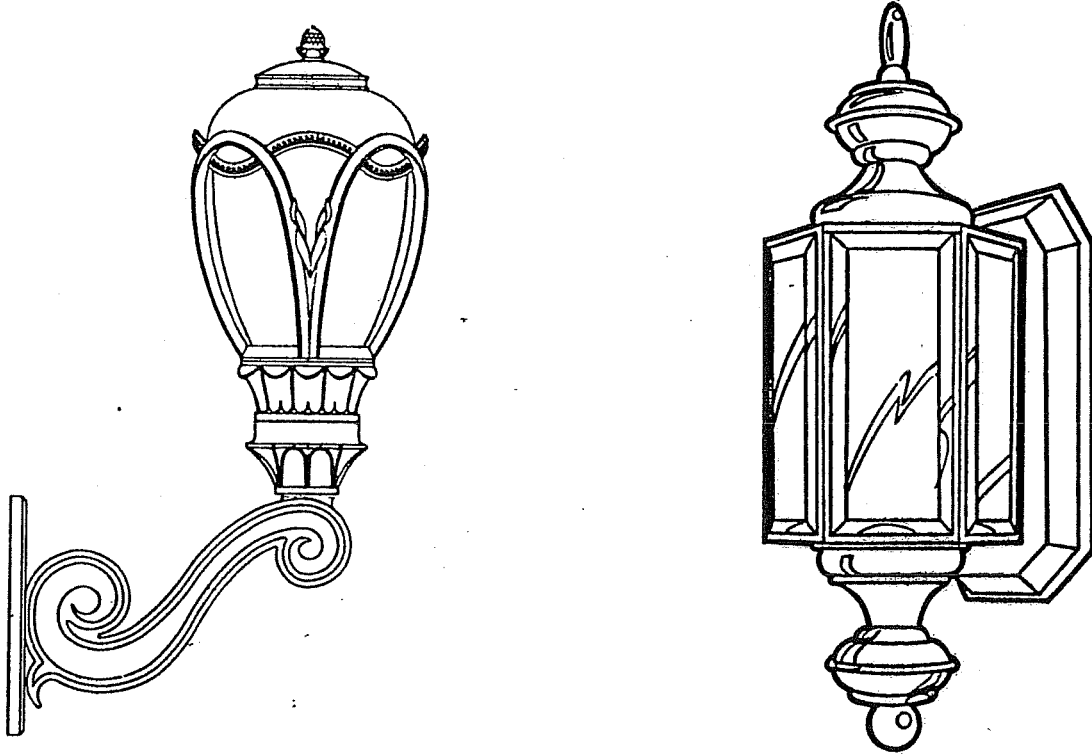
COMMERCIAL BLOCK - BRIDGE STREET

Bridge Street Commercial Block

Note:

The commercial block on Bridge street is the most active pedestrian block, especially at night. Existing street lights on the east side of the roadway do not adequately light the sidewalk on the opposite side. A collection different lighting solutions have been used on existing storefronts and retail signs. Some decorative sign lights have been installed. These should be encouraged. The use of utility or “security” style lights on storefronts should be discouraged.

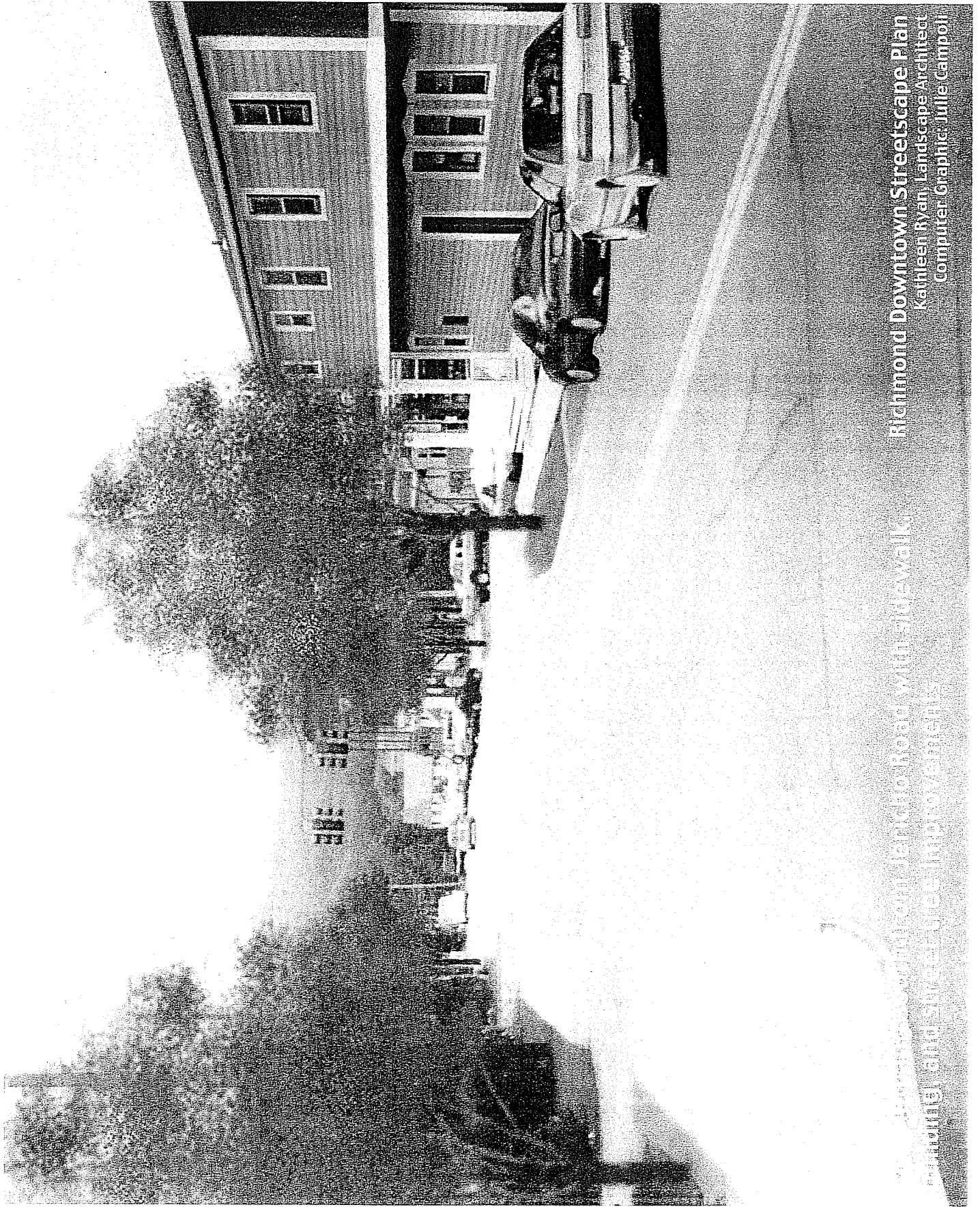
Although the narrow sidewalk does not lend itself to pole mounted fixtures, pedestrian lighting could be provided by the installation of six or seven matching, decorative period fixtures mounted on building columns, at the junction and at each end of the five commercial buildings. Fixtures should be installed at eight to ten feet above the sidewalk.



These are typical wall mounted period fixture that would be appropriate. There are many styles of luminaires available. Luminaire should feature low intensity true color rendering lamps and non-glaring cut-off reflectors. Low wattage incandescent lamps could supply enough light for sidewalk illumination

V. Streetscape Renderings

The following photographic renderings present several streetscape and architectural recommendations from this report as they would look from three different views toward the center of the downtown.



Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campbell

Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campbell

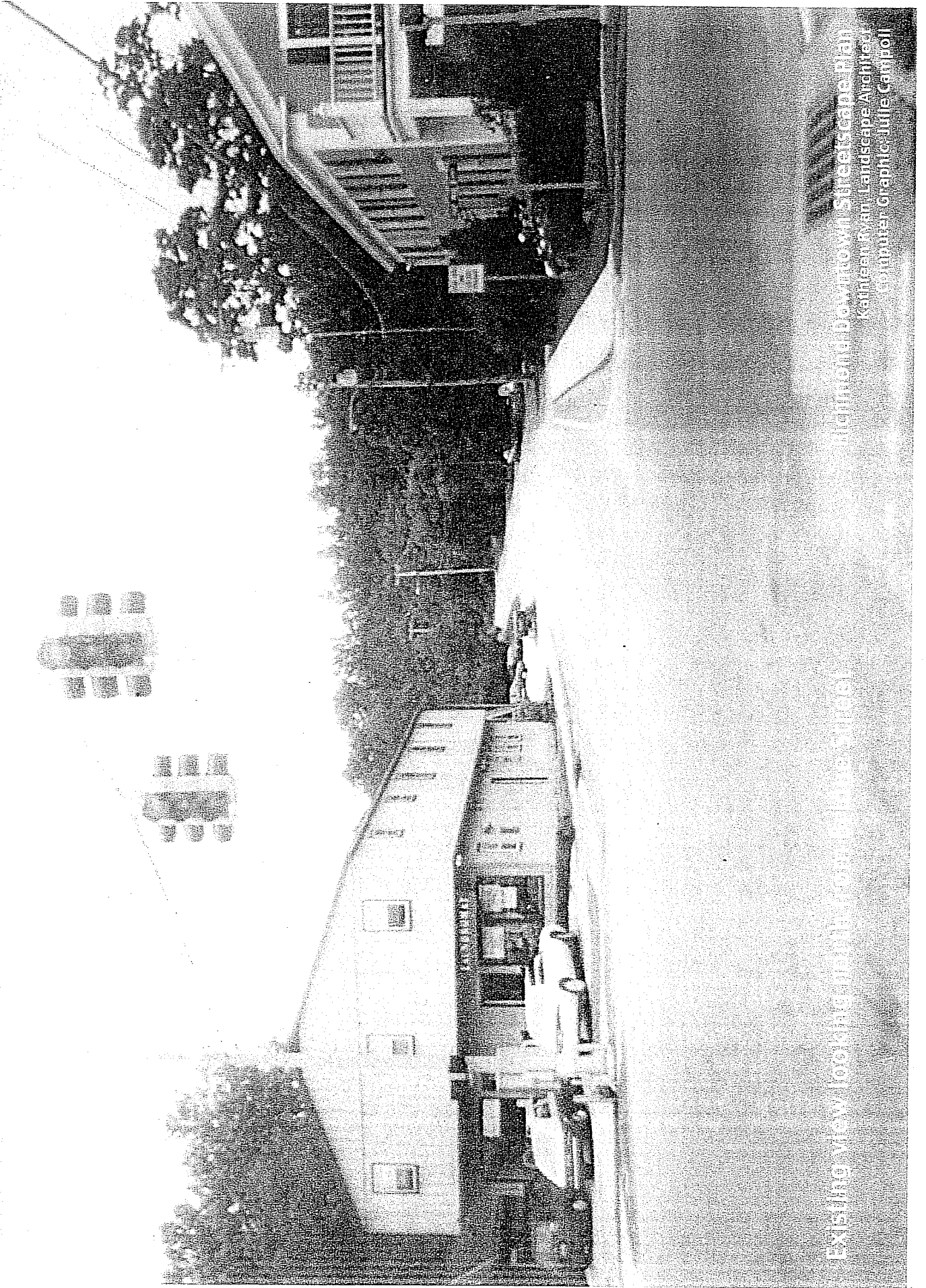


Existing view looking south from Jentcho Road

Richmond Downtown Streetscape Plan

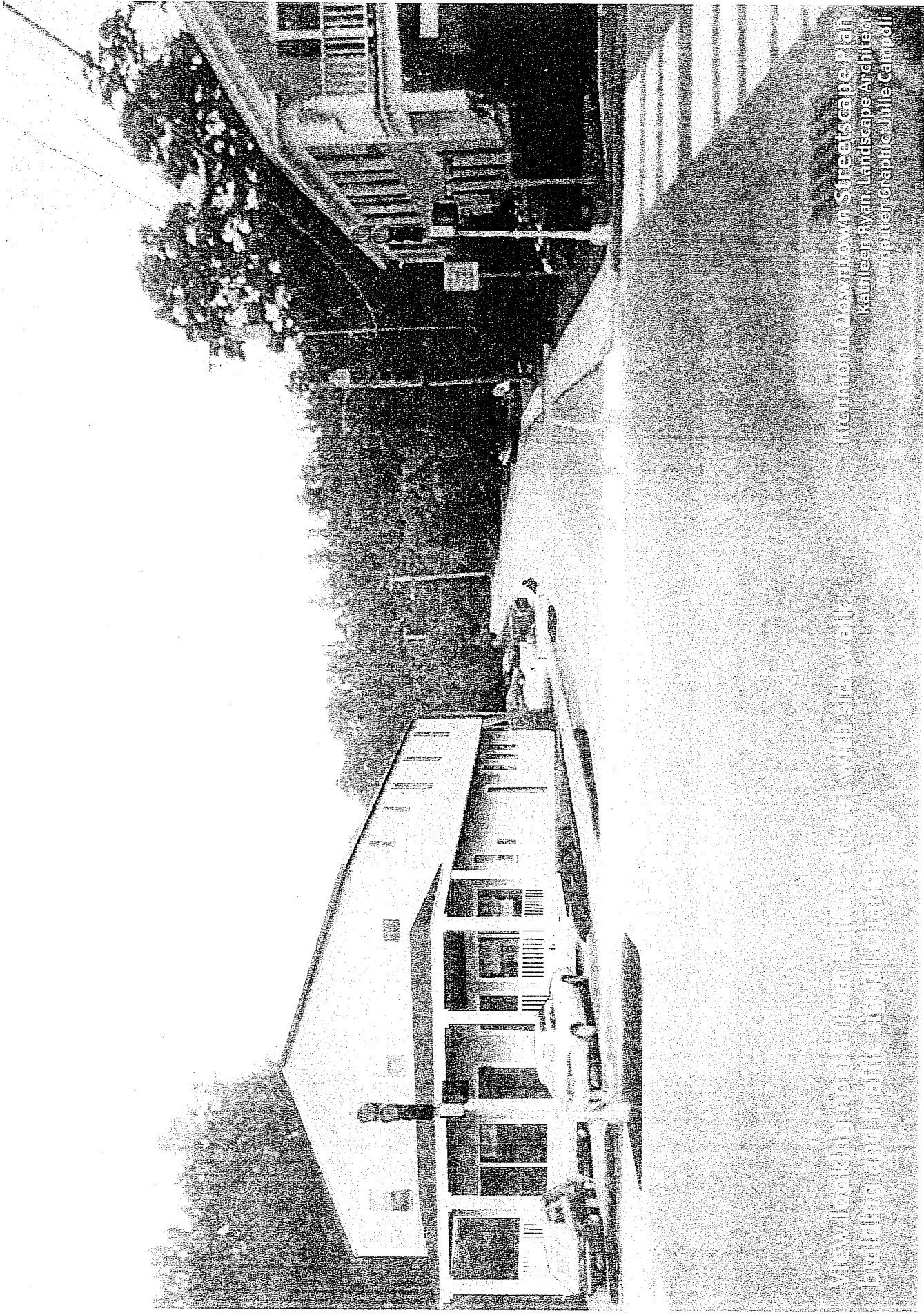
Kathleen Ryan, Landscape Architect

Computer Graphic: Julie Campoli



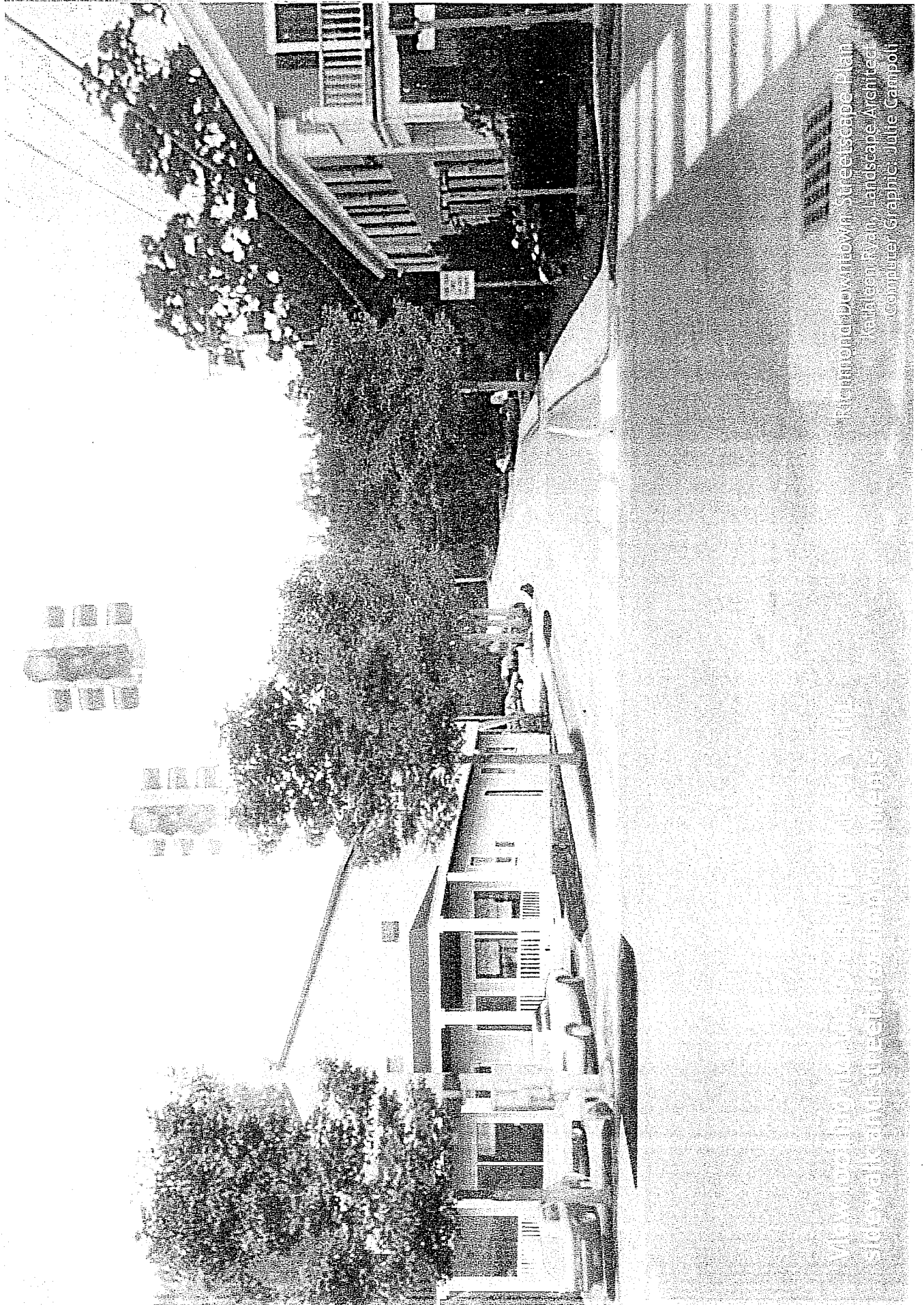
Existing view looking north from Baltimore Street

Highlighting Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campbell



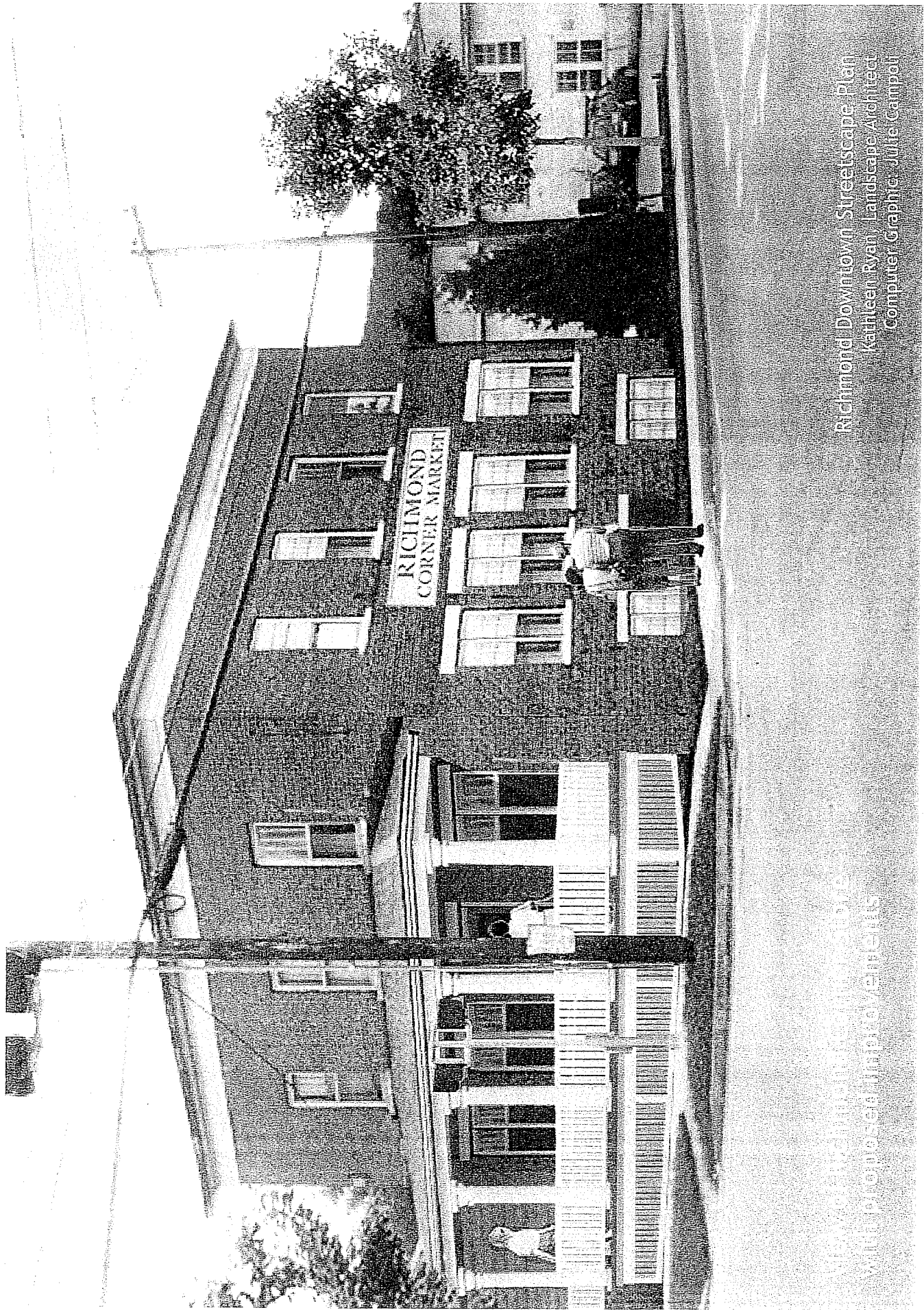
Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphics: Julie Campbell

View looking north from Burke Street with sidewalk
planting and traffic signal changes



View looking north along the sidewalk from the intersection of
sidewalk and street to the north.

Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campoli



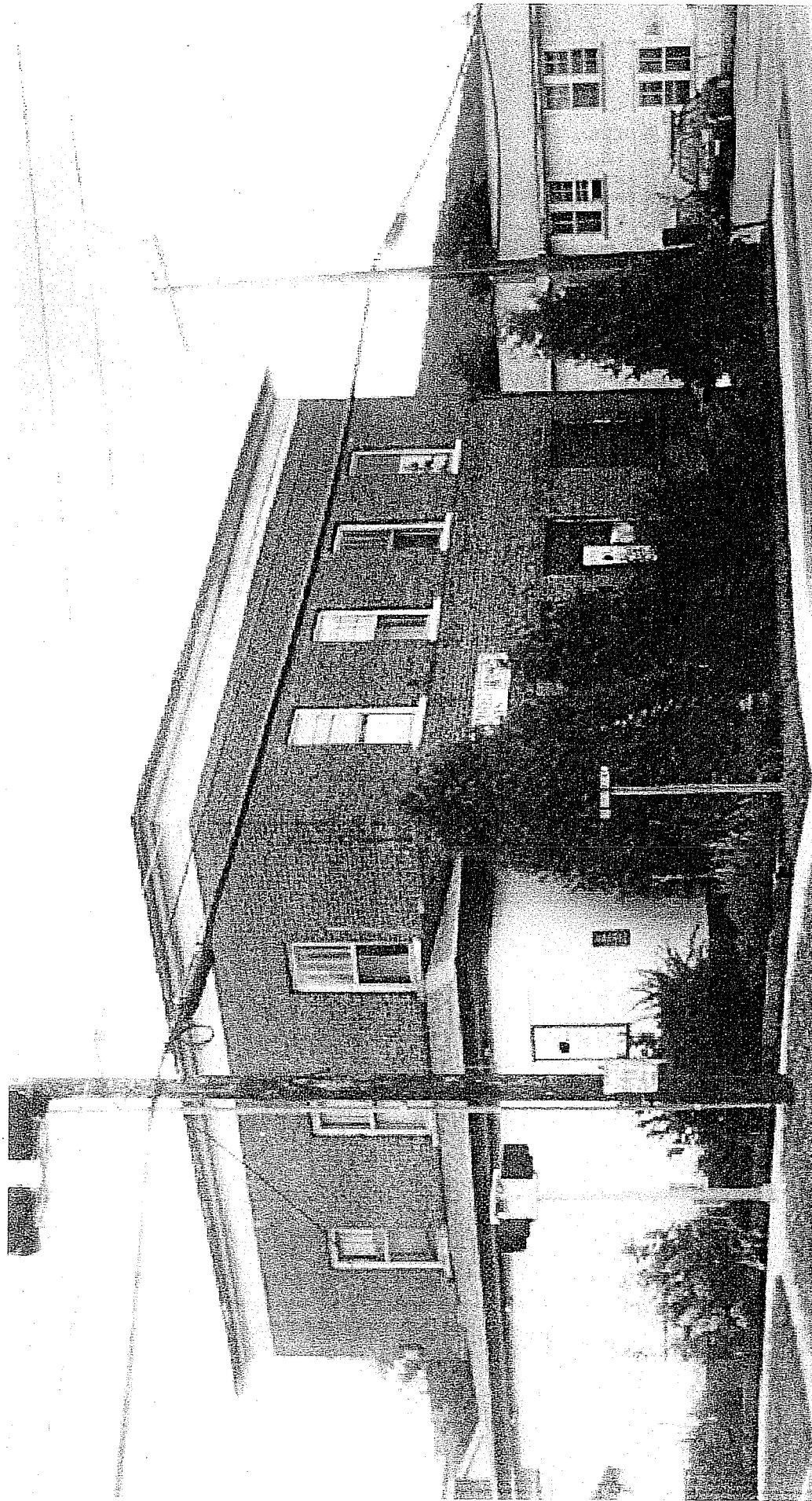
View of Richmond Corner Market
with proposed improvements

Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campoli



View of the three-story building with buried utility lines and building improvements.

Richmond Downtown Streetscape Plan
Kathleen Ryan, Landscape Architect
Computer Graphic: Julie Campbell



Richmond Downtown Streetscape Plan

Kathleen Ryan, Landscape Architect
Computer Graphics: Julie Campbell

VI. Costs

The following pages show the probable costs for various segments of the Richmond Downtown Streetscape Study. These are all site related costs. The cost of recommended facade improvements have not been estimated. Facade improvements shown vary from relatively low cost painting projects to fairly extensive residing and reconstruction work. In many cases full facade restoration costs can not be determined until outer siding material have been removed and the extent and condition of original detailing is known.

STREETSCAPE PROJECTS -		Bridge Street - Remaining Work, Main Street to			
		Railroad Street			
Item	Unit	Unit Price	Amount	Total per Item	Total
Public ROW					
Demolition and Dumping	cu. yd.	\$15	100	\$1,500	
Concrete Curb	ln. ft.	\$12	245	\$2,940	
4" Concrete Sidewalk	sq. yd.	\$30	140	\$4,200	
8" Concrete Sidewalk	sq. yd.	\$35	64	\$2,240	
Bituminous concrete patch	sq. yd.	\$15	100	\$1,500	
Grading	lump sum	\$500	1	\$500	
Pave Town parking lot	sq. yd.	\$11	900	\$9,900	
Stipe parking	space	\$5	20	\$100	
2 1/2"-3" Shade Trees	ea.	\$350	8	\$2,800	
Topsoil	cu yd.	\$15	8	\$120	
Fertilizer, seed and mulch	sq. ft.	\$0.30	400	\$120	
					\$25,920
Engineering and Design					\$2,592
Contingency					\$2,592
TOTAL					\$31,104

STREETSCAPE PROJECTS -		West Main Street - North Side, 1,100 ft.			
Item	Unit	Unit Price	Amount	Total per Item	Total
Public ROW					
Demolition and Dumping	cu. yd.	\$15	425	\$6,375	
Concrete Curb	ln. ft.	\$12	1,100	\$13,200	
Bituminous concrete patch	sq. yd	\$15	900	\$13,500	
New Catch Basin	ea	\$2,200	2	\$4,400	
Repair Roadway striping	sq. ft.	\$1	500	\$500	
2 1/2"-3" Shade Trees	ea.	\$350	9	\$3,150	
Topsoil	cu yd.	\$15	90	\$1,350	
Fertilizer, seed and mulch	sq. ft	\$0.30	4000	\$1,200	
Traffic control and signage for work in State Highway	per week	\$500	3	\$1,500	
					\$43,675
Engineering and Design					\$4,368
Contingency					\$4,368
TOTAL					\$52,410

STREETSCAPE PROJECTS - East Main Street - Both Sides					
Item	Unit	Unit Price	Amount	Total per Item	Totals
Public ROW					
Demolition and Dumping	cu. yd.	\$15	175	\$2,625	
8" Concrete Sidewalk	sq. yd.	\$35	41	\$1,435	
8" Concrete Sidewalk	sq. yd.	\$30	150	\$4,500	
Concrete Curb	ln. ft.	\$12	620	\$7,440	
New Catch Basin	ea	\$2,200	1	\$2,200	
Bituminous concrete patch	sq. yd	\$15	200	\$3,000	
2 1/2"-3" Shade Trees	ea.	\$350	6	\$2,100	
Topsoil	cu yd.	\$15	7	\$105	
Fertilizer, seed and mulch	sq. ft	\$0.30	400	\$120	
Traffic control and signage for work in State Highway	per week	\$500	2	\$1,000	
					\$24,525
Engineering and Design					\$2,453
Contingency					\$2,453
TOTAL					\$29,430

STREETSCAPE PROJECTS -		Jericho Road Segment - West side			
Item	Unit	Unit Price	Amount	Total per Item	Totals
Public ROW					
Demolition and Dumping	cu. yd.	\$15	225	\$3,375	
Concrete Curb	ln. ft.	\$12	350	\$4,200	
4" Concrete Sidewalk	sq. yd.	\$30	130	\$3,900	
8" Concrete Sidewalk	sq. yd.	\$35	33	\$1,155	
Bituminous concrete patch	sq. yd.	\$15	170	\$2,550	
2 1/2"-3" Shade Trees	ea.	\$350	7	\$2,450	
Topsoil	cu yd.	\$15	30	\$450	
Fertilizer, seed and mulch	sq. ft.	\$0.30	2000	\$600	
					\$18,680
Engineering and Design					\$1,868
Contingency					\$1,868
TOTAL					\$22,416
Private Parcel					
Demolition and Dumping	cu. yd.	\$15	90	\$1,350	
Grading	1/2 day	\$500	1	\$500	
Concrete Curb	ln. ft.	\$12	150	\$1,800	
2 1/2 " Bituminous concrete	sq. yd.	\$11	1027	\$11,297	
Pavement Striping	per space	\$5	16	\$80	
2 1/2"-3" Shade Trees		\$350	5	\$1,750	
Topsoil	cu yd.	\$15	19	\$285	
Fertilizer, seed and mulch	sq. ft.	\$0.30	1000	\$300	
					\$17,362
Engineering and Design					\$1,736
Contingency					\$1,736
TOTAL					\$20,834

STREETSCAPE PROJECTS - Railroad Parking Lot					
Item	Unit	Unit Price	Amount	Total per Item	Total
Topsoil Strip	cu. yd.	\$5	1400	\$7,000	
Common excavation	cu. yd.	\$10	425	\$4,250	
15" gravel base	cu. yd.	\$15	425	\$6,375	
2 1/2 " Bituminous concrete	sq. yd	\$11	460	\$5,060	
Pavement Striping	per space	\$5	43	\$215	
Roadway striping	sq ft.	\$1	40	\$40	
4 Ft. Aluminum Rail Fence	ln. ft.	\$30	350	\$10,500	
Signage	ea.	\$60	4	\$240	
2 1/2"-3" Shade Trees	ea.	\$350	6	\$2,100	
Topsoil spread (Use stripped topsoil)	cu yd.	\$15	25	\$375	
Fertilizer, seed and mulch	sq. ft	\$0.30	1500	\$450	
					\$29,605
Engineering and Design					\$2,961
Contingency					\$2,961
TOTAL					\$35,526

Funding Sources

LOCAL

- The Richmond Selectboard may chose to budget an annual amount for streetscape improvements such as curb and sidewalk. These funds could pay for small yearly projects or be allowed to accumulated to fund larger projects.
- A revolving loan fund could be created to fund facade improvements or site improvements on private property that benefit the town as a whole. Such a fund could be administered by the Richmond Business Association, the Historic Society, the Land Trust, or an coalition of these civic groups. The fund could accumulate moneys through donations from private individuals, groups or businesses and through fund raising activities.

STATE OF VERMONT

- The Agency of Transportation Enhancement Grant program is currently funding a sidewalk improvement project on West Main Street. Although highly competitive, the funds will continue to be available for at least the next five years for sidewalks, curb and other projects relating to transportation, including the restoration of buildings related to railroad history. Funding for building new sidewalks are said to have priority over repairing old sidewalks. Towns who have previously received funds will not be discriminated against in future funding years.
- Urban Forestry Grant. These funds, which are available through the Division of Forest, Parks and Recreation, pay for the planning and planting of trees in towns and villages. Grant applications for projects that have been well planned and that have made provision for the maintenance of trees on public lands are looked upon positively. In looking to add to their street tree inventory Richmond may do well to appoint a town Tree Warden (an often overlooked town office that is provided for in the state enabling legislation) who will take on the role of cataloging and keeping track of the well being of the Town's trees.
- Downtown Bill - The eligibility process by which a town is to receive the benefits and funds available through this legislation is still being written. The potential gain from this program seems to be primarily tax benefits for businesses who locate in downtowns with a small amount of direct funding available. It is anticipated, that in its early years, larger towns will see the primary advantage of this legislation. However, building a solid downtown development organization will put the Richmond in a good position to benefit in future years as the program develops.

FEDERAL

Few federal funding opportunities were identified. While historic preservation tax credits are available for restoring building listed on the National Register, the scope of the restoration work must be extremely comprehensive for an owner to benefit.

PRIVATE

The Orton Family Foundation recently started an award given annually to a store owner who runs and carries on the historic tradition of the Vermont country store. Other private foundations have

been known to fund historic preservation efforts that are run under the auspices of public or non-profit organizations.